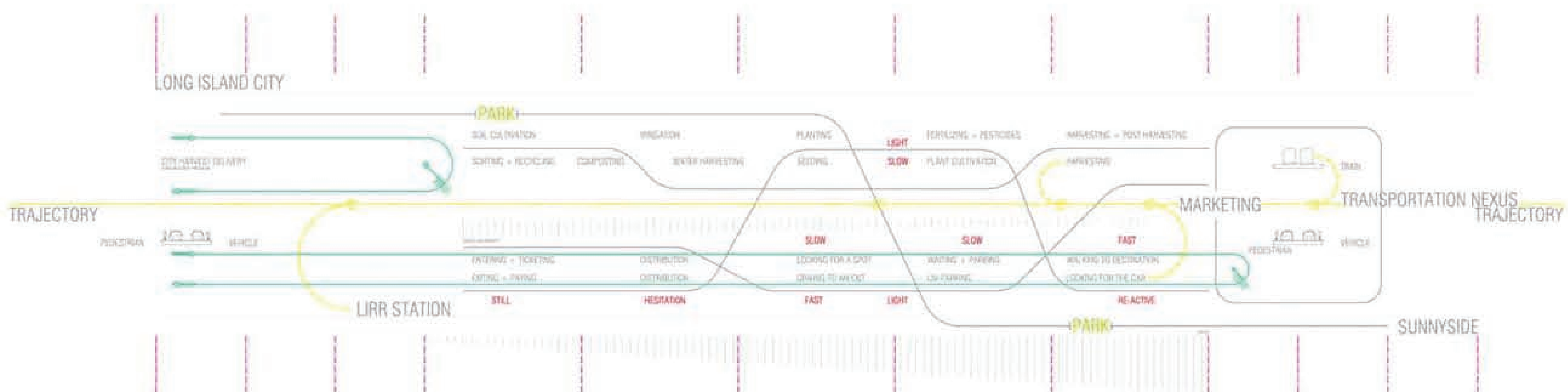
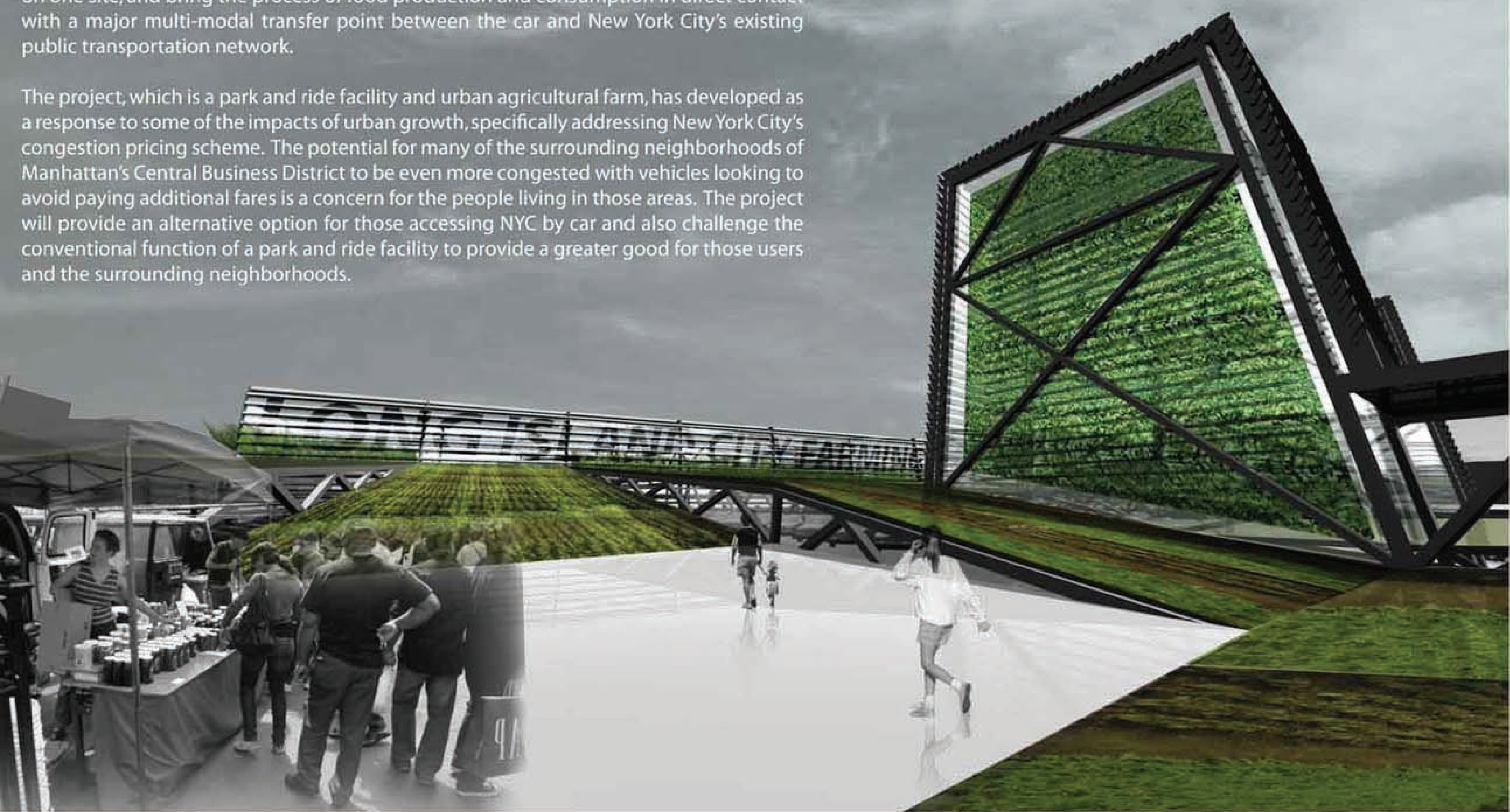


# LONG ISLAND CITY : FARMING PARK



All too often do we see land being taken away for parking and at the same time the reclamation of abandoned and vacant parking lots to turn into viable land, specifically farms in urban environments. The project attempts to combine these two typologies to co-exist on one site, and bring the process of food production and consumption in direct contact with a major multi-modal transfer point between the car and New York City's existing public transportation network.

The project, which is a park and ride facility and urban agricultural farm, has developed as a response to some of the impacts of urban growth, specifically addressing New York City's congestion pricing scheme. The potential for many of the surrounding neighborhoods of Manhattan's Central Business District to be even more congested with vehicles looking to avoid paying additional fares is a concern for the people living in those areas. The project will provide an alternative option for those accessing NYC by car and also challenge the conventional function of a park and ride facility to provide a greater good for those users and the surrounding neighborhoods.



The process of selecting a site is extremely important in that it needed to satisfy a number of certain urban criteria. Both programs want to be located adjacent to a major transportation nexus in addition to having a large amount of currently vacant land for farming. This adjacency facilitates the consumption of fresh produce at that multi-modal transfer point. The project is sited above the Sunnyside Rail Yards between Long Island City and Sunnyside, Queens; two growing neighborhoods of Manhattan.

This site connects the urban farm to a larger existing network of food distribution within NYC, specifically City Harvest, which is an organization that collects and redistributes wasted food from NYC's restaurants. The connection with City Harvest is the exchange of wasted food to be used as compost in the farm - for fresh food to be delivered to local markets and enhance food security. The community based infrastructure centralizes food production in this urban context and creates a continuous productive landscape out of this enormous vacant land.

This project, while designed specifically for New York City, can also be thought of as a prototype for how to address urban vehicular growth and food security in cities around the world. The site chosen in NYC is not much different than what can be found surrounding other large urban areas. Most large cities have some form of public transportation infrastructure, and accompanying that is always a rail yard; a potential farming site

